

BELL SYSTEM PRACTICES
Outside Plant Construction
and Maintenance

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POLE TRAILERS

LOADING AND BINDING

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1. GENERAL

- 1.01 This section replaces the parts of Section J6.621 that deal with loading and binding of loads on pole trailers.
- 1.02 This section covers methods of loading, binding and unloading poles using the standard pole trailers.
- 1.03 Since derricks and winches are used with pole trailers to load and unload poles, the sections on derricks, winches and wire rope shall be considered supplementing instructions. The sections on Pole Trailers—General and Pole Trailers—Equipment shall also be considered supplementing instructions.

2. SAFETY PRECAUTIONS

- 2.01 Before starting any work involving pole trailers, inspect the equipment to make sure that all parts are in place and in safe working condition.
- 2.02 When snaking a pole make sure that the pole does not catch on an obstruction which will cause it to swing around. If it is necessary to guide the pole, the guiding should be done at the rear end of the pole.

- 2.03 Chock the wheels of the trailer before detaching it from the truck, when the trailer is to be loaded with poles. When the trailer is loaded and parked it should also be chocked.
- 2.04 Whenever possible, use a truck to move the trailer into position for loading or unloading.
- 2.05 When coupling the trailer to the towing truck, keep hands clear of the towing eye and the towing hook.
- 2.06 After coupling the trailer to the towing truck, make sure that the towing hook is closed and that the latch is in the locked position.
- 2.07 Do not use the winch line to bind a load of poles.
- 2.08 Display lights, reflectors and flags on the end and sides of the pole load as required by local regulations.

3. LOADING POLES—GENERAL

3.01 The methods described in this section are some of those which may be used in loading poles. In the event that another method is used, the precautions which are given should be observed. The pole trailers are described in the section on Pole Trailers—General and the various binders and drawbars are described in the section on Pole Trailers—Equipment.

3.02 When loading poles on a two-wheel pole trailer, the position of each pole on the trailer should be such that the total load on the trailer is nearly balanced, but slightly tongue heavy so that the front end of the trailer can be lifted from the ground to attach the trailer to the towing truck. Test the load balance after every third pole is loaded. If the load is found to be too tongue heavy or tongue light, load the subsequent poles to balance the load. Where possible, load the poles with the butts forward in order that the shortest possible tongue length can be used.

3.03 If the length of poles to be loaded on the trailer will not permit the trailer tongue to be connected directly to the truck, the largest straight pole should be used as a king pole. It will always be necessary to use a king pole if the trailer is not equipped with an extensible tongue.

3.04 The king pole is loaded first and is placed directly over the tongue with the top of the pole toward the truck, the length of overhang should be as short as possible. The towing hook of the truck and the king pole are connected by using a drawbar. The binding of the king pole is covered in the section on Pole Trailers—Equipment.

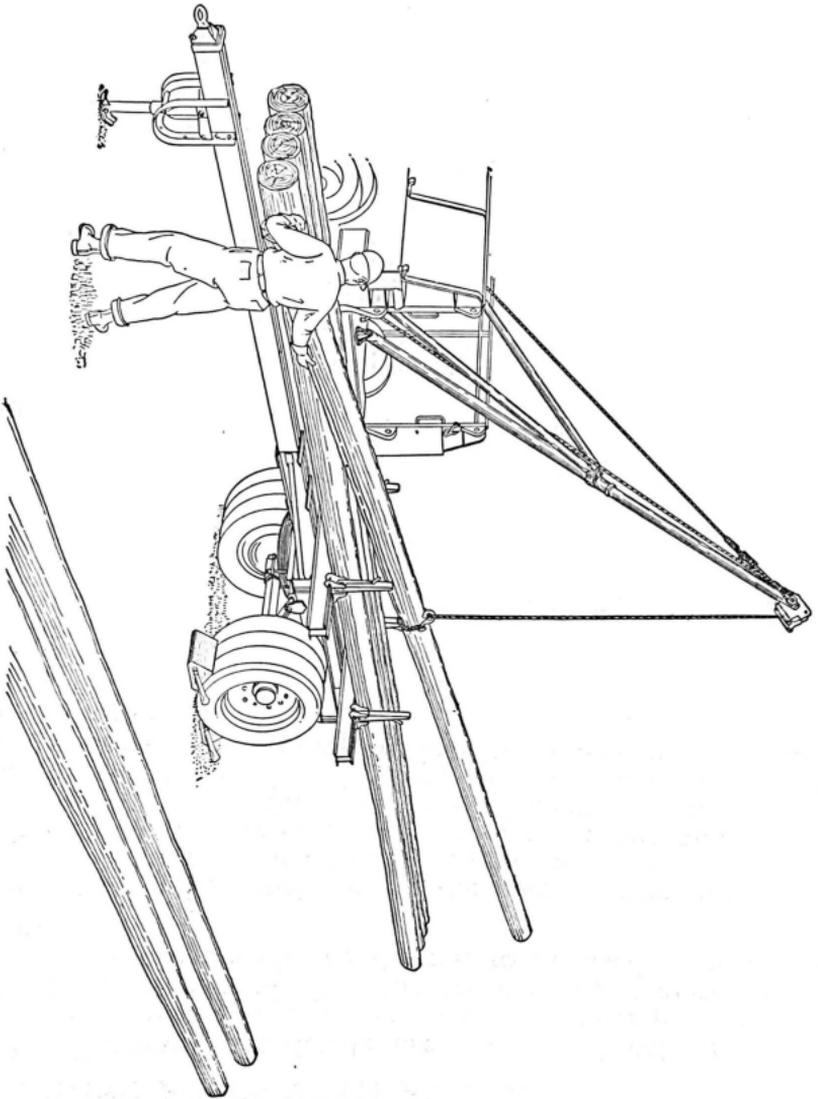
3.05 If the poles are to be delivered to stake locations and unloaded one at a time, load the poles so that the first poles to be delivered are on top of the load.

3.06 When using a trailer with an extensible tongue, keep the tongue as short as poles to be loaded will permit. Adjust the tongue by chocking the wheels of the trailer and then move the truck slowly until the proper length is reached.

4. LOADING POLES WITH POLE DERRICK

4.01 Whenever practicable use a standard pole derrick to load poles on the trailer. Place the trailer parallel to the poles to be loaded and place the truck at right angles to the trailer. This will permit the derrick to be used at its proper height.

4.02 Attach the winch line to the pole as near the balance point as possible and lift the pole onto the trailer. While loading the pole make sure that it does not swing and injure a workman or damage the truck or equipment. The load limitations of the type of derrick being used should be considered. See the practices on Pole Derricks for information on loads and capacities.



5. LOADING POLES USING SKIDS

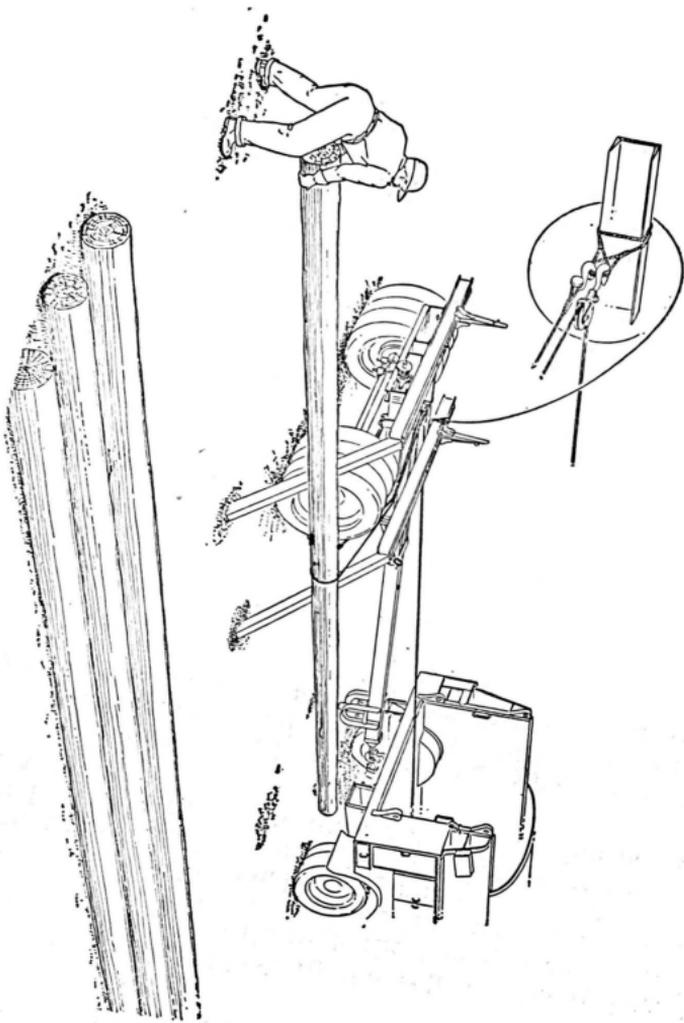
5.01 To load poles using skids the trailer may be left attached to the truck or the trailer can be unhooked from the truck but should be securely chocked.

5.02 Adjust the trailer tongue to the desired length for carrying the poles and place skids at the front and rear bolsters so that the poles may be rolled onto the bolsters. A skid which will handle all common telephone poles is a 4-inch, 7.7 lbs., steel I beam or its equivalent in strength (approximately 6x6 white oak beam). Arrange the skids so that they will not slip from the bolsters and the pole will clear the tires as they are rolled up the skids.

5.03 Attach a snatch block to the side of the trailer, then run the winch line through the block, over the pole to be loaded and then back to the rope holding the block. Then pull in the line using the winch and guide the pole up the skids to its place on the trailer. After the first few poles are loaded be careful that the winch line does not bind on the poles that are already loaded.

5.04 Skids may be used when loading poles from trailer height pole skids and the skids should be placed so that the pole will slide onto the skids rather than drop on them.

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OVER THE ROAD LOG SKIDDER

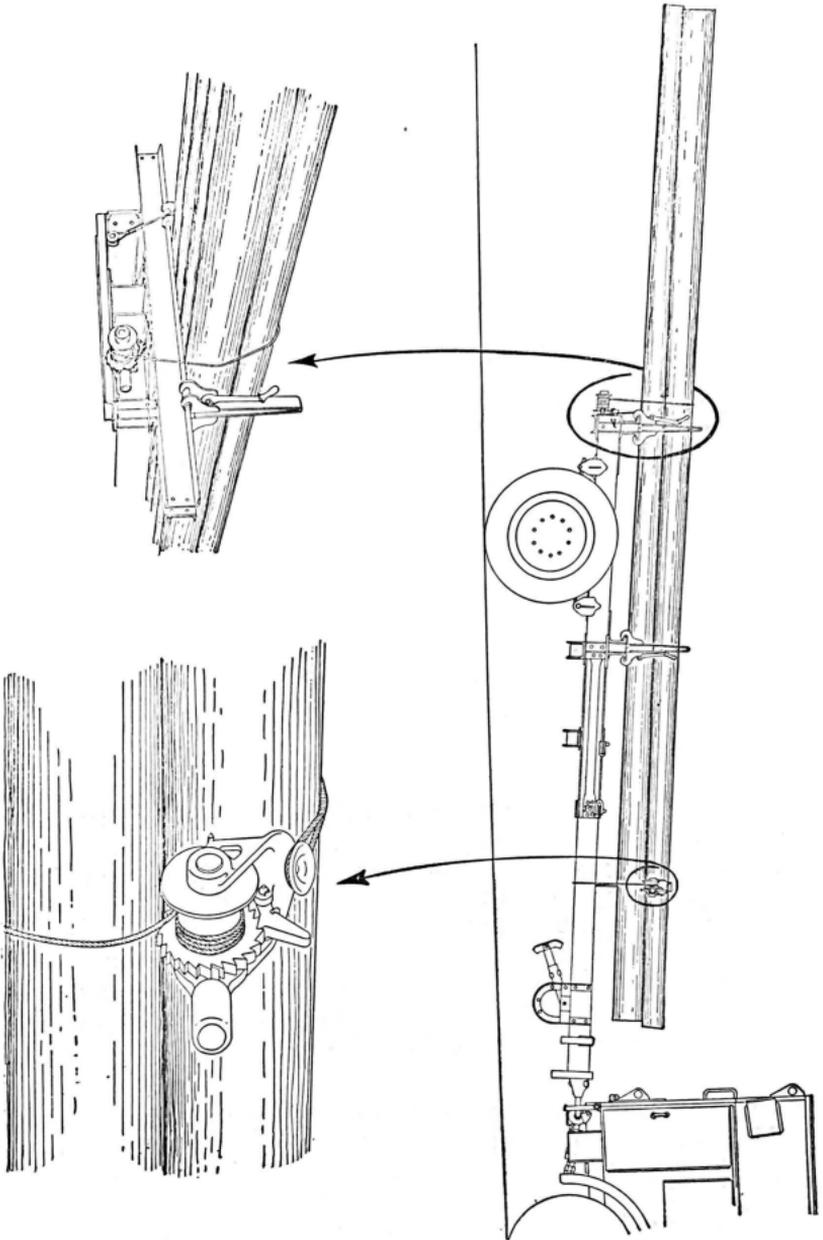


5.05 When loading poles onto a P3T or P8T trailer attach the snatch block to a stanchion.

6. BINDING POLES

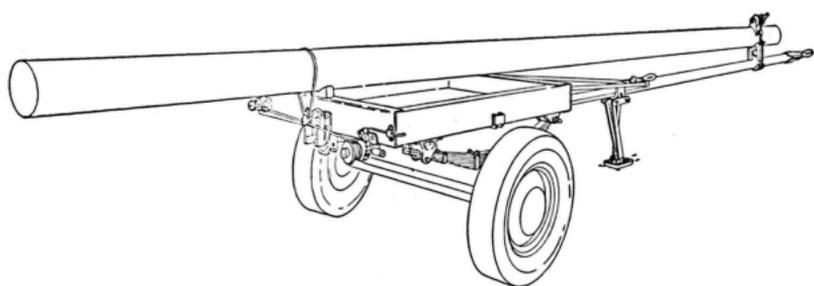
6.01 The methods of binding poles to a trailer will vary depending on the number of poles, the type of binders available, and the type of trailer being used. In general, loads can be classified into two categories; those on a trailer with an extensible tongue and those on a trailer which requires a king pole.

6.02 All loads of poles must be bound in two places, at the rear and at the front. If the rear bolster of the trailer is equipped with a load binding winch, pass the rope of the winch around and fasten the eye to the rope knob, then operate the winch to bind the load. At the front, the sling of a portable pole binder is wrapped around the entire load and the tongue or king pole and fastened on the rope knob on the binder. Tension is applied by the winch to bind the load. The following illustration shows a properly bound pole load.



6.03 In the event that a rear load binding winch is not supplied on the trailer, a portable pole binder should be used to bind the load. The rope sling should be passed around the trailer frame. Since the rope sling will pass around sharp corners it should be inspected prior to binding to be sure that the sling will not fail.

6.04 When one pole is carried on a trailer the king pole binders are used to bind the pole. When these binders are not supplied, the following figure shows the method of binding the pole using the rear load binding winch and a portable pole binder.

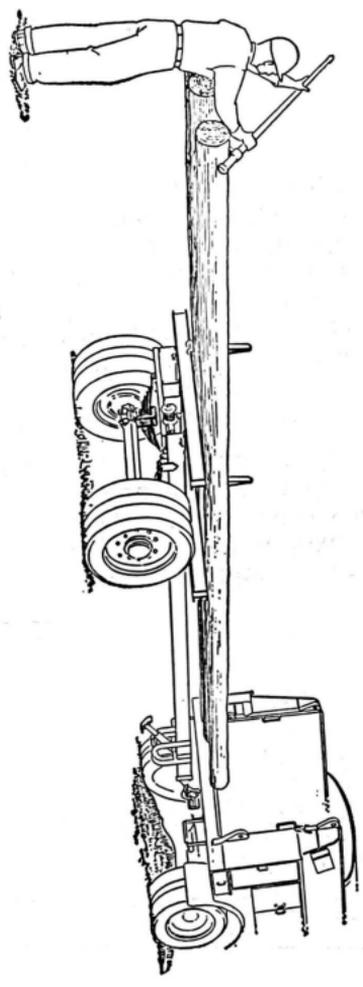


7. UNLOADING POLES

7.01 If all the poles are to be unloaded at the same location, it is generally advisable to unload them with a pole derrick. If the poles are to be unloaded a short distance apart, the derrick may generally be left up while travelling between points where the poles are to be unloaded.

7.02 The poles can be removed from the load by rolling each pole over the side of the trailer. Skids may be required depending on the length of the pole and the surface condition of the ground. Do not remove the trailer stanchions until the top layer of poles is unloaded. Make sure that the remaining poles are not dislodged when the pole is moved, which may remove the support. Place a temporary binding around the poles which are to remain before removing the stanchions and rolling a pole off the load. Keep the temporary binder in place as each pole is rolled off the trailer.

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7.03 If the poles are to be distributed to stake locations near an existing pole line, the poles may be unloaded by snaking out of the load. Attach one end of a line to the pole to be unloaded and snub the other end of the line around a standing pole. The distance between the load and the standing pole should be enough to avoid an accident due to the pole bouncing, and the pull should be so that the pole is as nearly as possible in line with the rope pulling the pole out of the line. Drive the truck forward slowly to pull the pole out of the load. The pole will be dragged out of the load and drop to the ground when the pole clears the load.

